

## 2.0

# PROJECT DESCRIPTION

The following information is based on the November 2010 Draft Fairview Corners Residential Specific Plan, accompanying list of errata prepared by EMC Planning Group, and related project application materials. A copy of the draft Specific Plan is available at the County of San Benito Planning Department.

## 2.1 PROJECT OVERVIEW

The project applicant, Fairview Corners, LLC (project applicant or developer) has applied to San Benito County for approval of the Fairview Corners Residential Specific Plan (“Specific Plan”) and related entitlements (“proposed project”). The applicant seeks to develop a residential project on an approximately 60-acre project site, which is located southeast of the City of Hollister, in unincorporated San Benito County. Among other things, this project is intended to build a residential community that is integrated with and supports the recently approved Gavilan College San Benito Campus.

The proposed project would amend the San Benito County General Plan and change the land use designation from “Rural” to “FVC-SP”, and would create the Fairview Corners Specific Plan (FVC-SP) zoning district to guide the development of the project site. The Specific Plan describes the proposed residential community as well as provides a comprehensive planning and regulatory framework for development of the proposed project. To ensure the necessary flexibility to respond to market demand and community needs, the Specific Plan provides for a range of potential densities that may be developed as well as a variety of potential housing types, including secondary units. However, a maximum of 220 primary dwelling units would be permitted on the project site. In addition, the project proposes park and recreational uses, including active parks, open space and a pedestrian and bikeway network, as well as related on- and off-site project infrastructure. For a more detailed description of the project components, see Section 2.3, Project Characteristics, below.

## 2.2 PROJECT LOCATION AND SETTING

### ***Regional Location***

The project would be constructed on an approximately 60-acre site, located southeast of the City of Hollister, in unincorporated San Benito County (project site). See Figure 1, Regional Location. San Benito County is situated in the California Coast Range Geomorphic Province, south of San Francisco Bay and east of Monterey Bay. San Benito County is bordered by Santa Clara and Santa Cruz Counties to the north, by Merced and Fresno Counties to the east, and by Monterey County to the west and south. All of San Benito County is unincorporated with the exception of the City of Hollister and the City of San Juan Bautista, both of which are located near the northern County line.

### ***Project Site and Immediate Vicinity***

The project site is located approximately one-quarter mile to the north of Airline Highway (SR 25) and directly east of Fairview Road. The project site is located outside of the Hollister city limit and sphere of influence, but is located within Hollister's General Plan Planning Area Boundary. See Figure 2, Project Vicinity, for the project site in relationship to the City of Hollister's jurisdictional boundaries and the vicinity road network.

The project site consists of one parcel of land (APN 025-190-068). The site is currently undeveloped land that is used to cultivate barley. The land is annually disced and periodically grazed by cattle. The western, northern, and eastern sides of the project site are fenced. There are no trails, roads or other improvements on the site. The project site does not support overhead or underground utility transmission lines. However, a water pump station, operated by Sunnyslope County Water District (Sunnyslope), is located in the northwestern corner of the site along Fairview Road. The pump is connected to an agricultural water line that currently serves the project site. Refer to Figure 4, Aerial Photograph, for an aerial view of the project site and its surroundings, and Figure 5, Site Photographs, for the existing conditions on the project site.

The San Andreas fault lies approximately eight miles southwest of the project site. The Calaveras fault, a branch of the San Andreas fault, bisects the City of Hollister and lies about 1.25 miles southwest of the project site. The project site is located within the Alquist-Priolo Earthquake Fault Zone due to the proximity of the Tres Pinos fault, a branch of the Calaveras fault that is generally considered to be potentially active. A geotechnical investigation conducted of the project site shows that a 35-foot-wide trace of the Tres Pinos fault is mapped on the site; the location of this trace is shown on Figure 6, Topography and Earthquake Fault Building Exclusion Zone.

The project site's topography consists of undulating hills with an overall gradual elevation change of about 45 feet from east to west. A former stock pond is located in a ground depression near the northeast corner of the site. The highest elevation is the crest of a hill near the center of the site and the lowest points are in the southwest corner near Fairview Road and in the northeast corner in the vicinity of the former stock pond. The site rises from Fairview Road to the crest of the hill located approximately 1,100 feet east of Fairview Road. Slopes on the site vary from zero to about 10 percent, as illustrated by Figure 5, Site Photographs, and inferred from the information presented in Figure 6, Topography and Proposed Earthquake Fault Building Exclusion Zone.

Existing drainage patterns on the site follow the topography and generally flow in three directions: west of the crest of the hill, the site drains toward Fairview Road; to the east, the site drains to a low point in the site's northeastern corner; and along the southern boundary, the crest of the hill is interrupted by a saddle, which causes drainage to flow southward toward the adjacent property. Refer to Figure 7, Existing Drainage.

## ***Surrounding Land Uses***

The project site is bounded by rural residential uses and grassland along Old Ranch Road to the north, rural residential uses and grassland along Harbern Way to the east, a small family-owned farm to the southeast, the approved but undeveloped Gavilan College San Benito Campus project to the south, and Fairview Road and the Cielo Vista single-family residential subdivision to the west. The Ridgemark Golf and Country Club, which includes a gated residential community, is located further to the south across Airline Highway. The approved but undeveloped 292-acre Santana Ranch project site is located approximately one mile to the north, on Fairview Road. The undeveloped area located between the approved Santana Ranch project and the project site is identified by the County as the Central Fairview Study Area. The Award Homes residential subdivision, on the west side of Fairview Road, just north of the Cielo Vista subdivision, has been approved by the City of Hollister, but has not yet been developed (Refer to Figure 3, Vicinity Surrounding Uses).

## **2.3 PROJECT CHARACTERISTICS**

The proposed Fairview Corners Residential Specific Plan establishes a detailed framework for development of the project, which would comprise residential uses, as well as park and recreational facilities, and related on- and off-site infrastructure.

## ***Residential Uses***

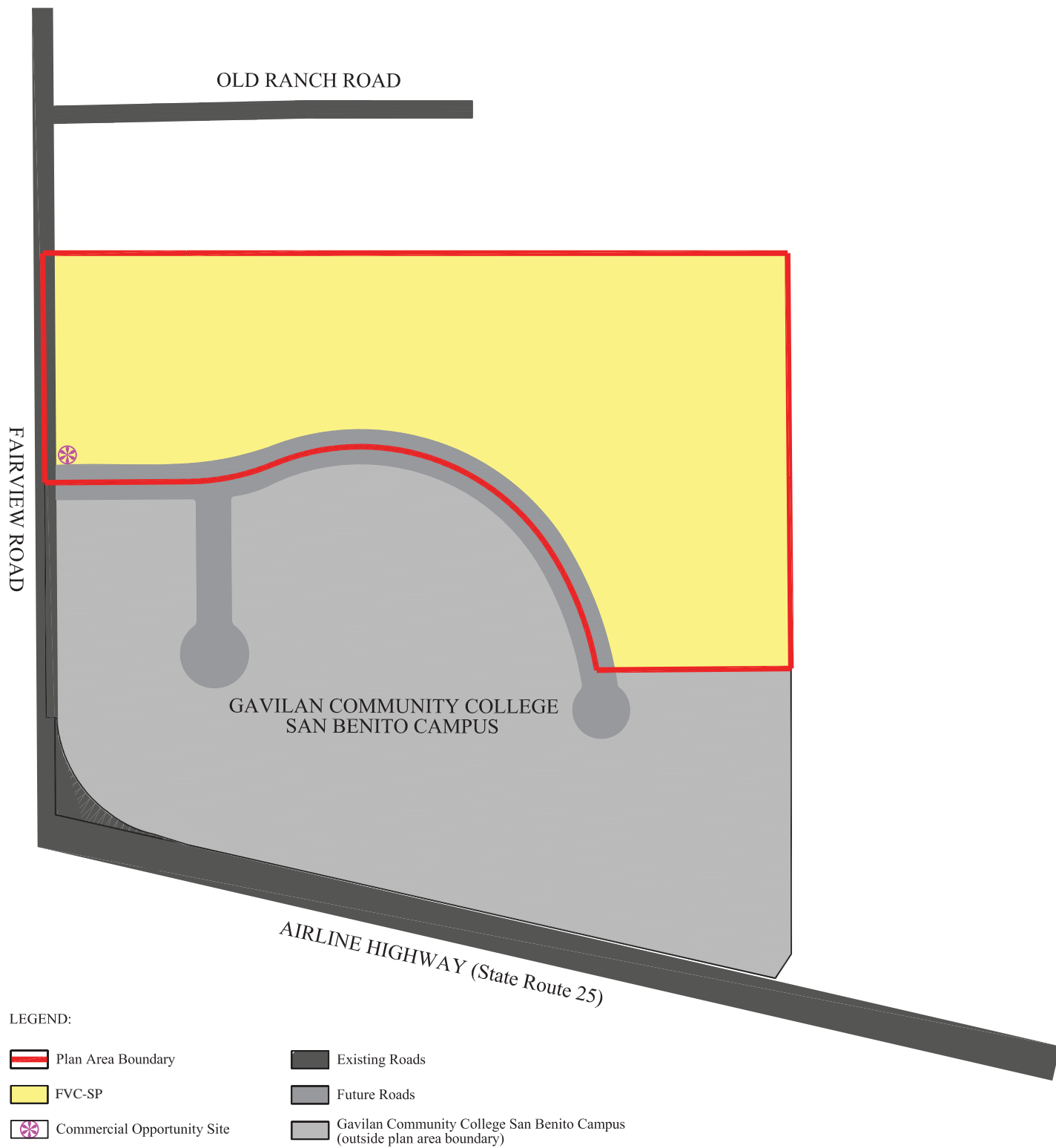
### **Land Uses, Density and Unit Count**

The Specific Plan is designed to be a flexible, comprehensive planning document, which also provides the regulatory framework within which development would occur, taking into account market demand and community needs. Accordingly, the Specific Plan provides that a range of potential densities may be developed, from a gross density of one dwelling unit per five acres to a gross density of 3.6 dwelling units per acre, corresponding with a minimum of 12 primary dwelling units to a maximum of 220 primary dwelling units that would be permitted to be developed on the project site under the Specific Plan. The maximum allowed overall density would be less than 4.0 dwelling units per acre, at approximately 3.6 units per acre. The size of the lots may vary from a minimum of 4,000 square feet up to five acres.

The project proposes to provide a variety of housing types to address the needs of a range of economic segments in San Benito County, from apartments and small lot cluster homes to single-family ranchettes on larger lots. Secondary units also are proposed, which could range from apartment flats over garages to detached units on single-family lots. In addition, as described further below, the project proposes park and recreational uses, including pocket parks, open space areas and a pedestrian and bikeway network, as well as related on- and off-site project infrastructure. As shown in [Figure 9, Proposed Land Use](#), the project site would have a General Plan land use designation of “Fairview Corners-Specific Plan (FVC-SP).” Table 1, Permitted Land Uses, presents the proposed uses permitted under the FVC-SP land use designation.

### **Development Flexibility**

A maximum of 220 primary dwelling units would be permitted on the project site. Housing types allowed by the proposed project would vary from small lot cluster homes to single-family ranchettes. However, the proposed project also includes policy provisions that would allow higher density duplexes, multiple dwelling unit groups and apartment uses, with a corresponding reduction in the number of lower density residential units to maintain the maximum number of units at 220 units. Three conceptual examples of lotting plans are provided in Appendix B of the Specific Plan to demonstrate the flexibility of the proposed development approach for the project site. The three Conceptual Lotting Plans are illustrated here by [Figures 10, 11, and 12 \(Conceptual Lotting Plan A, Conceptual Lotting Plan B, and Conceptual Lotting Plan C, respectively\)](#), and are summarized in [Table 2, Examples of Development Flexibility: Conceptual Lotting Plans](#).



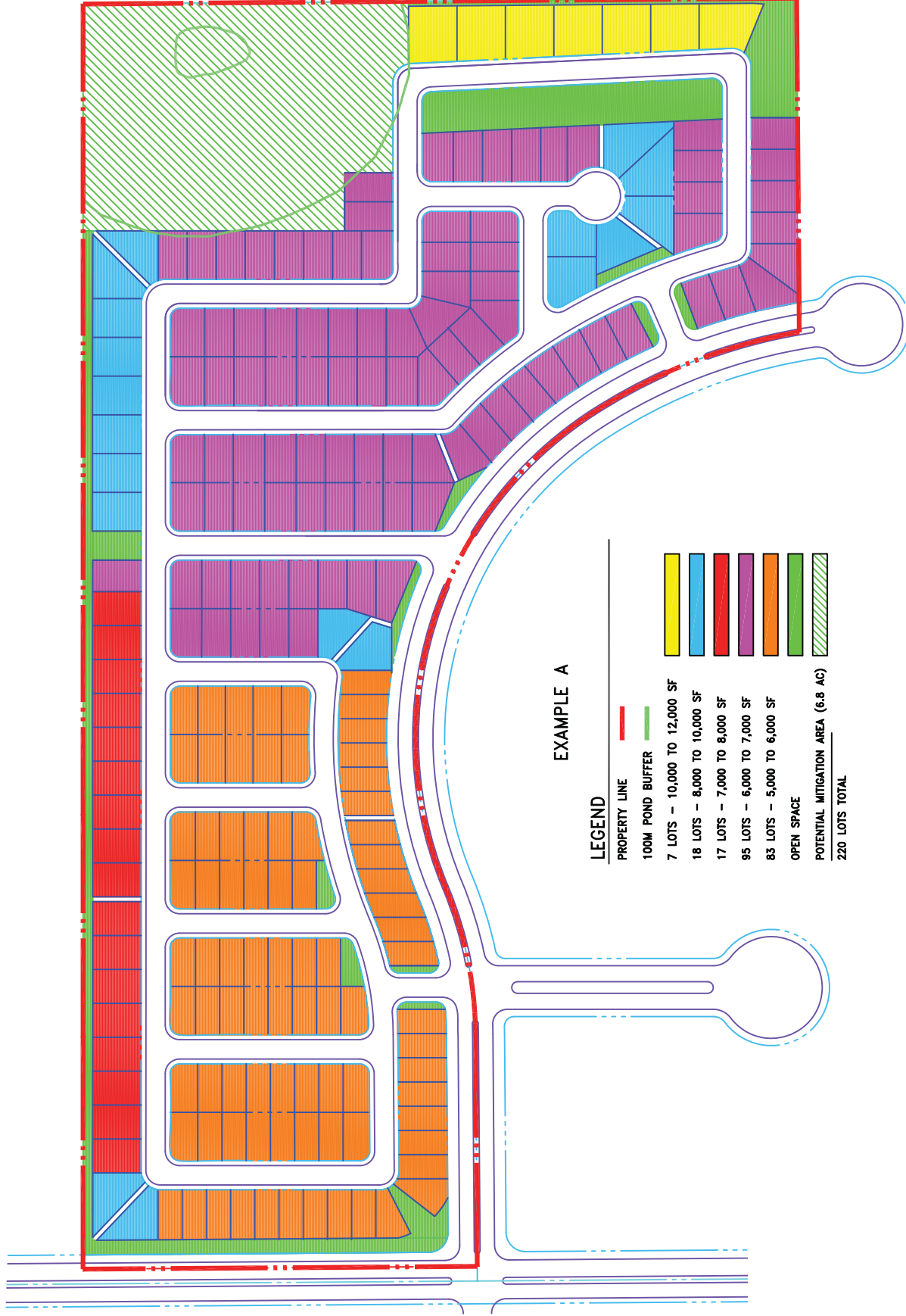
Source: EMC Planning Group 2010  
For illustrative purposes only

Figure 9

## Proposed Land Use

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*



Source: EMC Planning Group 2010

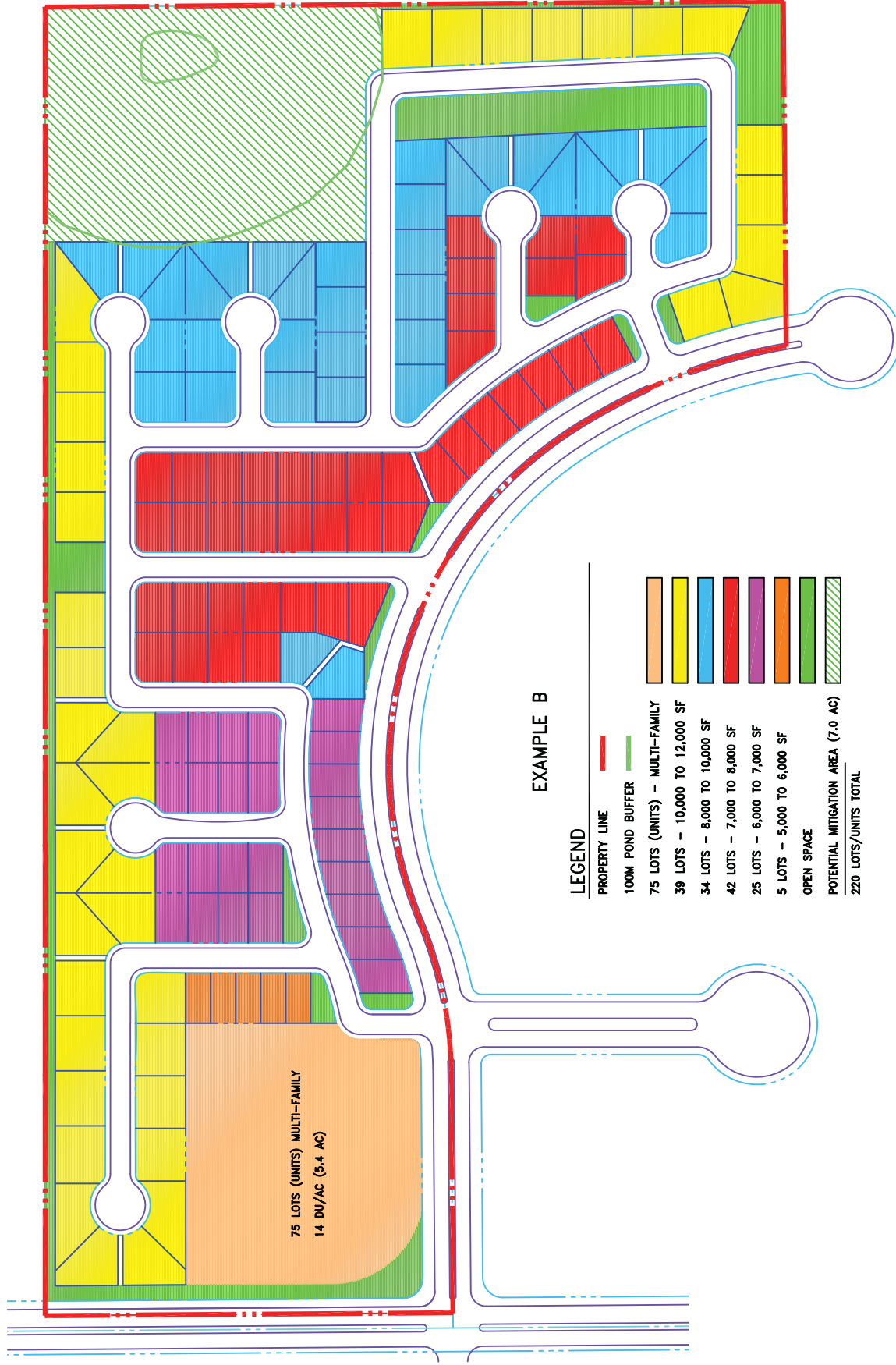
Figure 10

# Conceptual Lotting Plan A

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

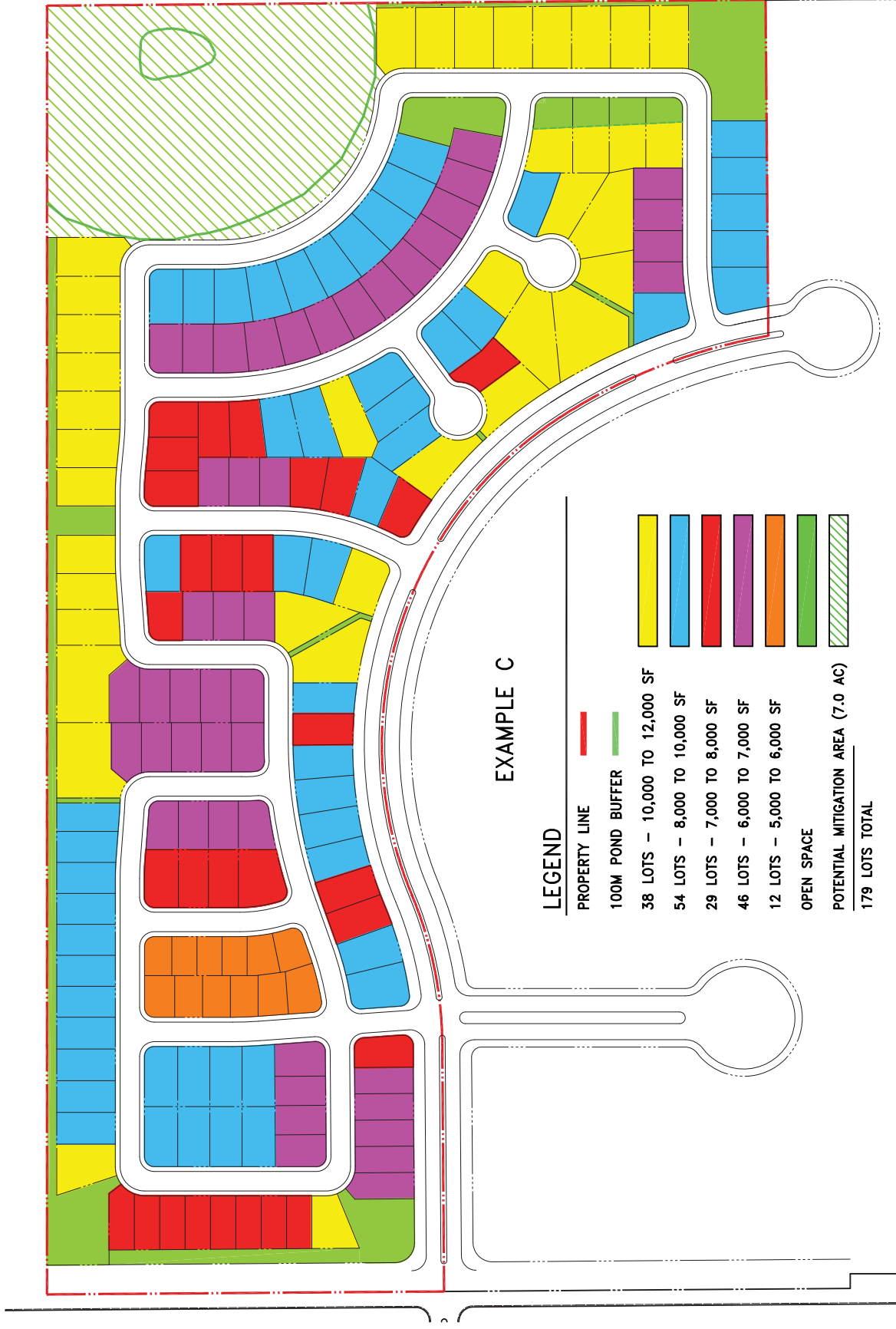




Source: EMC Planning Group 2010

Figure 11  
Conceptual Lotting Plan B  
Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*



Source: EMC Planning Group 2010,  
Ruggeri-Jensen-Azar 2009

Figure 12  
Conceptual Lotting Plan C  
Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

**Table 1 Permitted and Conditional Land Uses**

<b>Classification</b>	<b>Acceptable Uses</b>
<b>Principal Uses Permitted on Residential Lots</b>	<ul style="list-style-type: none"> <li>a. Detached single family dwelling;</li> <li>b. Attached single, duet, duplex, triplex, fourplex dwelling;</li> <li>c. Other uses (prescribed by State law as required to be permitted uses in residential districts; i.e. pre-emptive uses).</li> </ul>
<b>Complementary Uses Permitted on Lots Designated on the Subdivision Map for a Specific Use</b>	<ul style="list-style-type: none"> <li>a. Multi-family dwelling(s);</li> <li>b. Common area landscaping and open space;</li> <li>c. Utility(ies) or service facility(ies) serving the residential development;</li> <li>d. Private road(s), driveway(s), parking lot(s), or trail(s).</li> </ul>
<b>Accessory Uses Permitted on Residential Lots</b>	<ul style="list-style-type: none"> <li>a. Guesthouse (as the term is defined in Policy LU-4.1 #3) or secondary dwelling unit, not to exceed one per lot and as further specified herein;</li> <li>b. Detached garage, carport, workshop, storage building, pool house, patio cover, deck, trellis, gazebo, play structure, or greenhouse;</li> <li>c. Non-commercial garden, orchard, or other horticulture;</li> <li>d. Home occupation meeting the standards of San Benito County Code section 25.29.090 et sec.;</li> <li>e. Personal recreational vehicle or boat storage, but not within front yards and road side yards.</li> </ul>
<b>Conditional Uses</b>	<ul style="list-style-type: none"> <li>a. Single-family residential lots of one acre or larger;</li> <li>b. Caretaker units;</li> <li>c. Day cares, elderly care homes, group homes and similar uses serving over six persons;</li> <li>d. Park, recreational community building(s) or facility(ies);</li> <li>e. Non-commercial garden, orchard, or other horticulture in the absence of a principal use;</li> <li>f. Non-habitable accessory building in the absence of a principal use;</li> <li>g. Other uses (prescribed by State law) conditionally permitted in residential districts. See also Item c.</li> </ul>

*Source:* EMC Planning Group, Inc. (2011)

**Table 2 Examples of Development Flexibility: Conceptual Lotting Plans**

<b>Example A</b>	<b>Example B</b>	<b>Example C</b>	<b>Lot Sizes/Density</b>
--	75 MFU	--	Multi-Family (14 du/ac)
7 SFU	39 SFU	38 SFU	Single-Family (10,000 – 12,000 s.f.)
18 SFU	34 SFU	54 SFU	Single-Family (8,000 – 10,000 s.f.)
17 SFU	42 SFU	29 SFU	Single-Family (7,000 – 8,000 s.f.)
95 SFU	25 SFU	46 SFU	Single-Family (6,000 – 7,000 s.f.)
83 SFU	5 SFU	12 SFU	Single-Family (5,000 – 6,000 s.f.)
<b>220 SFU</b>	<b>220</b> <b>(75 MFU, 145 SFU)</b>	<b>179 SFU</b>	--

*Source:* EMC Planning Group, Inc. (2010)

## Commercial Uses

The Specific Plan also includes policies generally supportive of future neighborhood-commercial uses on a maximum of five acres of land on the project site near Fairview Road, and acknowledges the anticipated compatibility of such potential uses. However, there is no application to develop any such neighborhood-commercial uses at this time. The Specific Plan, if approved, would not allow development of neighborhood-commercial uses at this time. To implement such uses, the developer would need to seek and obtain a Specific Plan amendment and other related discretionary approvals after compliance with project-level CEQA review. Accordingly, this EIR does not evaluate the environmental impacts of any such neighborhood-commercial uses as part of the project studied herein.

## Parks and Open Space

The Specific Plan proposes to include parkland and open space to serve the project's residents, as well as to facilitate connections with the adjoining Gavilan College San Benito Campus.

Specifically, the project would meet the County's park and open space requirement, which mandates the provision of five acres of parkland per 1,000 new residents. According to Census information obtained through the California Department of Finance, as of January 2010, the average household size in San Benito County is 3.081. Assuming the maximum build-out of 220 units (678 residents at 3.081 persons per unit), the project would need to provide 3.4 acres of open space and parks on-site. In the alternative, the developer could pay the applicable in-lieu fee — particularly given the close proximity of extensive park and open space features planned on the adjoining Gavilan College San Benito Campus site — which the County could then use to help fund park and open space facilities in other areas in the County. The proposed project intends to meet the County standard, by providing a minimum of 3.4 acres of parkland or by the provision of a lesser amount of parkland on the site in combination with the payment of in-lieu fees.

The proposed project includes policies for the provision of active and passive recreational and open space opportunities. Active parks could take the form of “pocket parks” located strategically throughout the neighborhoods, as well as a potential par course. Passive and/or active open space uses could occur in certain areas of the project site that would otherwise be reserved for the geologic fault area (near the 35-foot Tres Pinos fault) that accommodates a “building exclusion zone,” as well as a potential habitat mitigation area near the former stock pond (located in the northeastern corner of the site), depending on resource agency requirements and the ultimate site plan. If required by these resource agencies, it is anticipated that the habitat mitigation area would consist of the former stock pond and a buffer zone (100-meter radius) around the former pond. A visual example of the potential locations and configuration of parkland and open space is provided in [Figure 13, Open Space Diagram](#). As indicated by [Figure 13](#), buffer areas are also located on the project site between residential lots and adjacent uses to the north and south.

Also, the proposed project includes policies for an open space and park trail system in a loop configuration around the outer boundaries or within the interior of the project site, designed to interconnect the residential neighborhoods with each other and the adjoining Gavilan College San Benito Campus. The ultimate design of the proposed park and open space features would be finalized as part of the first tentative map application process, as reflected in an approved Open Space and Parks Master Plan.

## ***Traffic and Circulation***

### **Circulation**

The project's circulation network includes roads, pedestrian and bicycle connections, and potential facilities that could support future public transit along Fairview Road. The proposed primary access to the project site would be an eastern extension of Cielo Vista Drive, intersecting Fairview Road and extending into the project site as the major collector street. This extension would provide access to both the project site and the adjoining Gavilan College San Benito Campus, and would follow the boundary between the two sites.

The major collector, the Cielo Vista Drive extension, would be built by either the Fairview Corners master/individual developers or Gavilan College District, depending on which area is developed first. The Cielo Vista Drive extension is expected to be constructed in at least three (3) stages as the respective developments proceed, commencing from the western portion of the site (near Fairview) and progressing east. Because the costs of constructing the Cielo Vista Drive extension are anticipated to be shared between Fairview Corners and the Gavilan College San Benito Campus projects and both projects have responsibility to ensure adequate access in connection with their respective projects, the entity and timing to initiate construction of the roadway will depend on which project commences construction first. [Figure 14, Circulation Diagram](#), presents the extent of the shared roadway system on the project site. This diagram assumes full buildout of the Gavilan College San Benito Campus and the proposed project. Should all or a portion of the proposed project commence construction prior to the Gavilan College San Benito Campus, the Fairview Corners master/individual developers would be required to take the lead in constructing the portion of the Cielo Vista Drive extension to serve the Plan Area on a phase by phase basis.

The Cielo Vista Drive extension would be a public street serving the Plan Area and the Gavilan College San Benito Campus, and at full buildout, would include curbs, gutters, separated sidewalks, street lighting, utilities, street trees, and additional landscape strips and medians in accordance with applicable County standards and requirements as set forth in the Specific Plan. If completed to serve both projects, the median would straddle the property line shared by these projects. [Figure 15, Typical Future Cielo Vista Drive Extension Street Section](#), indicates a preliminary roadway layout to serve the full buildout of the Plan Area and the Gavilan College San Benito Campus, which consists of two 12-foot travel lanes in each direction, with a six-foot, Class II bike lane in each direction, a 12-foot center median, four- to six-foot landscaping (planter) strips on each side of the street, and seven-foot sidewalks on each side of the street. These improvements will be phased and scaled based upon construction of the Plan Area and the Gavilan College San Benito Campus.



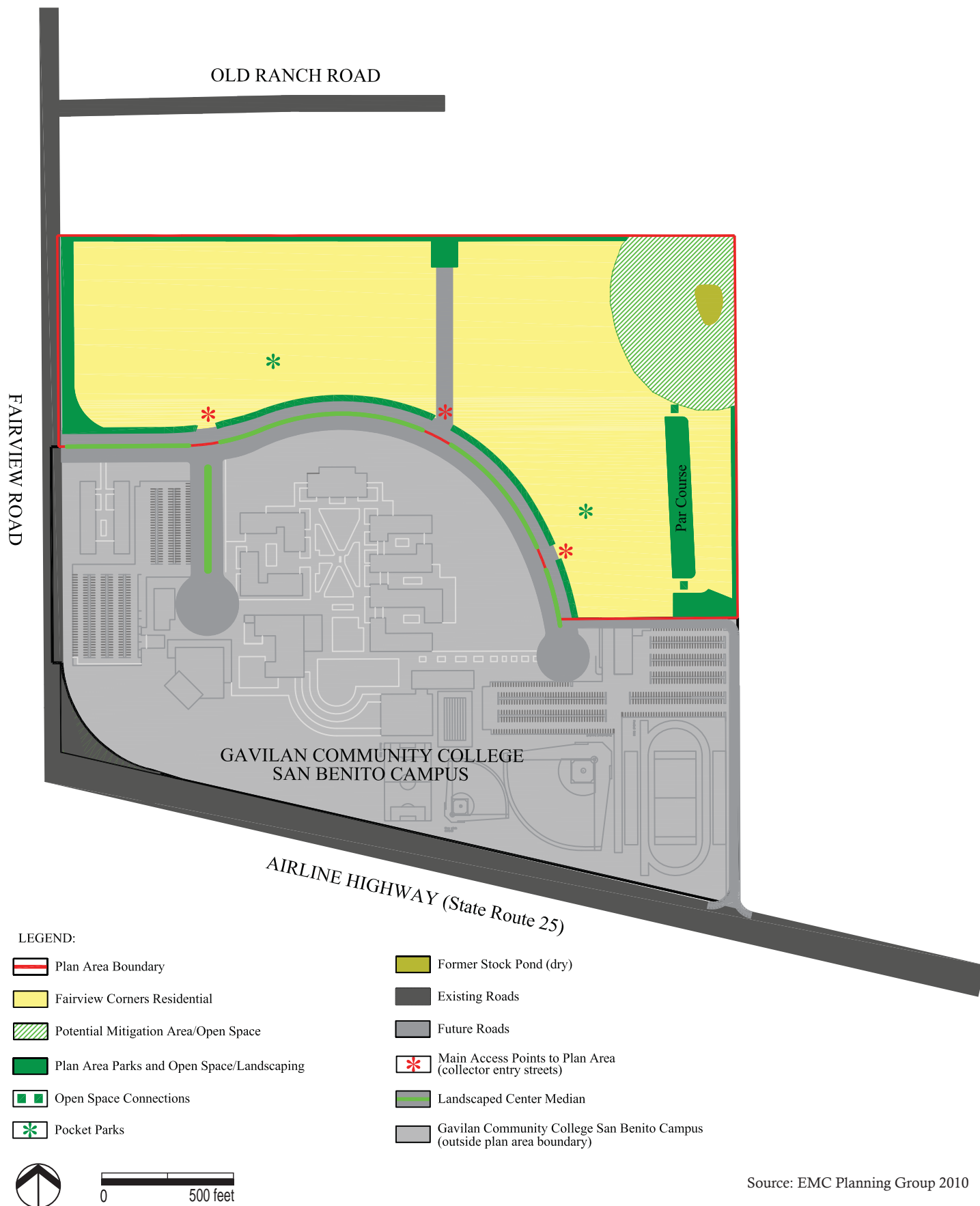
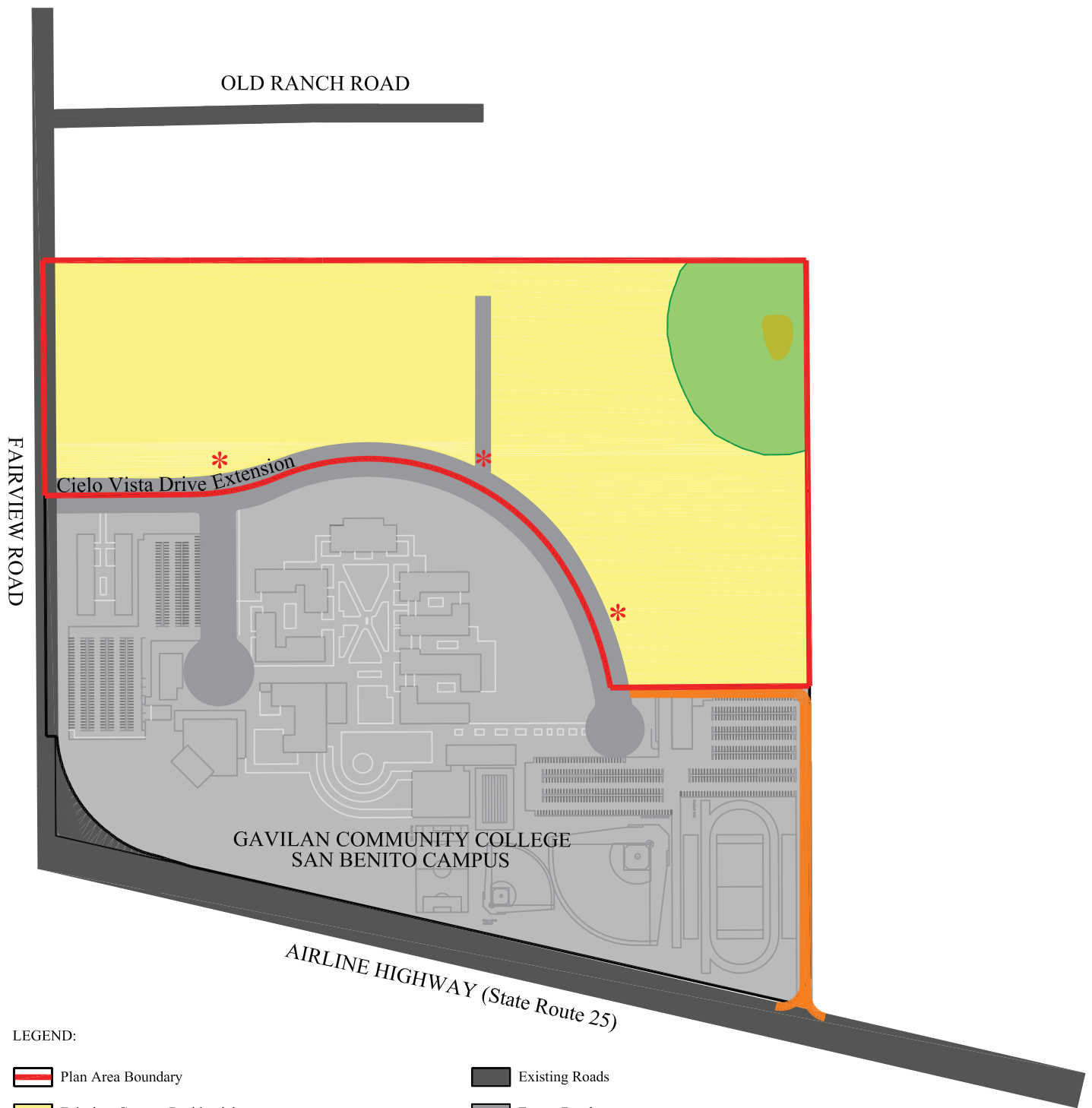


Figure 13

## Open Space Diagram

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*



LEGEND:

- |  |  |
|--|--|
|  Plan Area Boundary                           |  Existing Roads   |
|  Fairview Corners Residential                 |  Future Roads   |
|  Former Stock Pond (dry)                      |  Emergency Vehicle Access (EVA)   |
|  Potential 100m Buffer of Pond (in Plan Area) |  Main Access Points to Plan Area (collector entry streets)                |
|  Potential Onsite Mitigation for CTS          |  Gavilan Community College San Benito Campus (outside plan area boundary) |



Source: EMC Planning Group 2010  
For illustrative purposes only

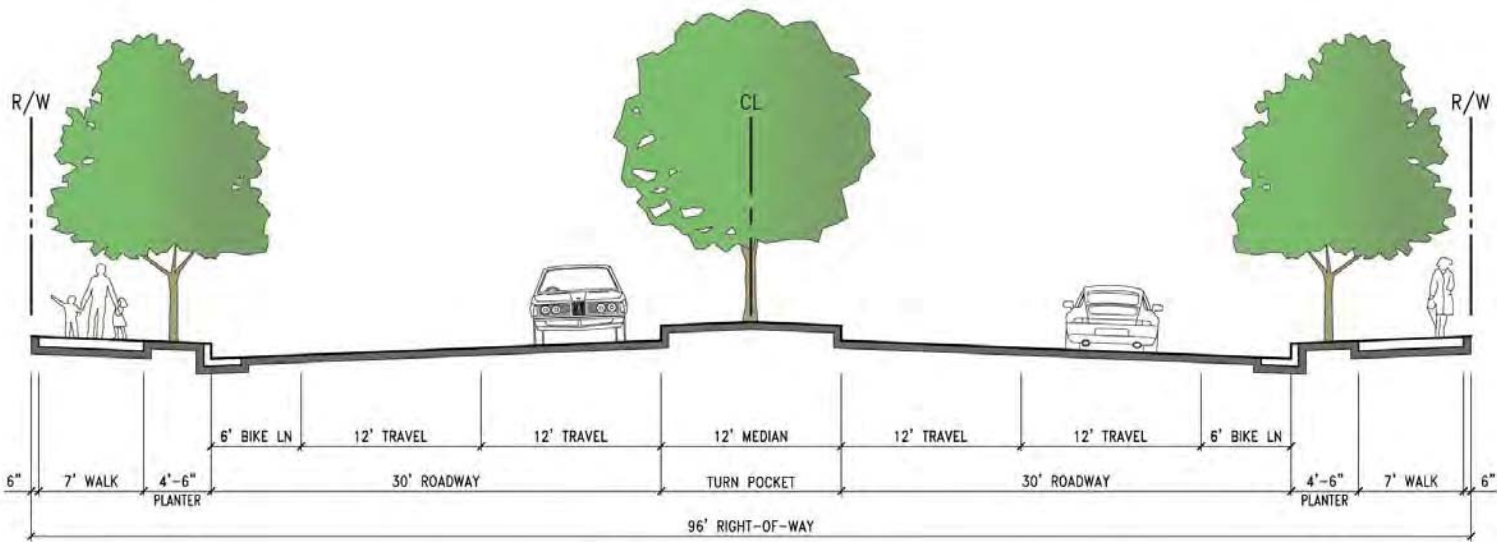
Figure 14

## Circulation Diagram

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

# Typical Future Cielo Vista Drive Extension Street Section



Not to scale

Source: EMC Planning Group 2010, Kier & Wright 2008

Figure 15

*This side intentionally left blank.*

Proposed internal circulation is assumed to be a loop roadway network with some cul-de-sacs. The on-site circulation network would be designed to integrate pedestrian, bicycle and vehicular modes of transportation, and to maintain open space at the end of cul-de-sacs that provide pedestrian and bicycle connectivity. Design and construction of the proposed roadways would be subject to the review and approval of the San Benito County Public Works Department and would be required to comply with applicable provisions of the Specific Plan as well as County standards and other requirements. Conceptual examples of possible street layouts are set forth in the conceptual lotting plans illustrated by Figures 10-12. The details of the on-site circulation plan would be finalized as part of the first tentative or parcel map application process (as the case may be), in accordance with an approved Street Improvement and Streetscape Master Plan for the project.

Minor collector neighborhood streets extending from the Cielo Vista Drive extension may include design features that incorporate best management practices (BMPs) for storm drainage systems. These features may be incorporated into the design subject to approval by the County Public Works Department, and would transition to standard residential streets at appropriate locations. At buildout of the project, these internal streets would be two-lane roads with separated sidewalks and landscape strips on both sides of the street. See [Figure 16, Typical Collector Entry and Residential Street Sections](#). Traffic calming features, such as bulb-outs and neck-downs, may be integrated into the project design, as appropriate, particularly along the Cielo Vista Drive extension to promote a safe pedestrian-friendly environment.

### **Off-Site Improvements**

The Specific Plan proposes a number of off-site circulation improvements that are necessary to accommodate the potential development within the project site. The developer would offer to dedicate the necessary right-of-way along the Fairview Road frontage of the project site to allow for the future widening of Fairview Road from two to four lanes. The developer would also construct frontage improvements on Fairview Road, concurrent with the first phase of development. As indicated by [Figure 17, Existing and Typical Future Fairview Road Section](#), the Specific Plan requires two 12-foot northbound lanes, two 12-foot southbound lanes, appropriate turn lanes, a 6-foot bike lane in each direction, a 14-foot raised median, as well as sidewalks, shoulders, a meandering pedestrian/bicycle path, curbs and gutters on Fairview Road.

In addition, the Specific Plan includes policies that require improvements to the Cielo Vista Drive extension, including the installation of a traffic signal if warranted. With the construction of the project, the east leg of Cielo Vista Drive would be built, a northbound shared through/right-turn lane would be added to Fairview Road at this intersection, a southbound left-turn lane would be added to Fairview Road at this intersection, and the west leg of Cielo Vista

Drive eastbound would be re-stripped to provide a left-turn lane and a shared through/right-turn lane. A discussion of project-related impacts and mitigation measures for the intersection of Fairview Road/Cielo Vista Drive extension is found in Section 3.14, Traffic and Circulation, of this Draft EIR. The cumulative effects of project-related traffic and its impact to area roadways and intersections are also discussed in Section 3.14 of this Draft EIR.

### **Secondary Access**

The proposed project includes policy provisions for secondary access. All new roadways would be constructed to County and other applicable standards and requirements, including load and access requirements for emergency vehicles. The proposed project intends to utilize the emergency vehicle access (EVA) route planned for by the adjoining Gavilan College San Benito Campus project, as shown on the Circulation Diagram ([Figure 14](#)).

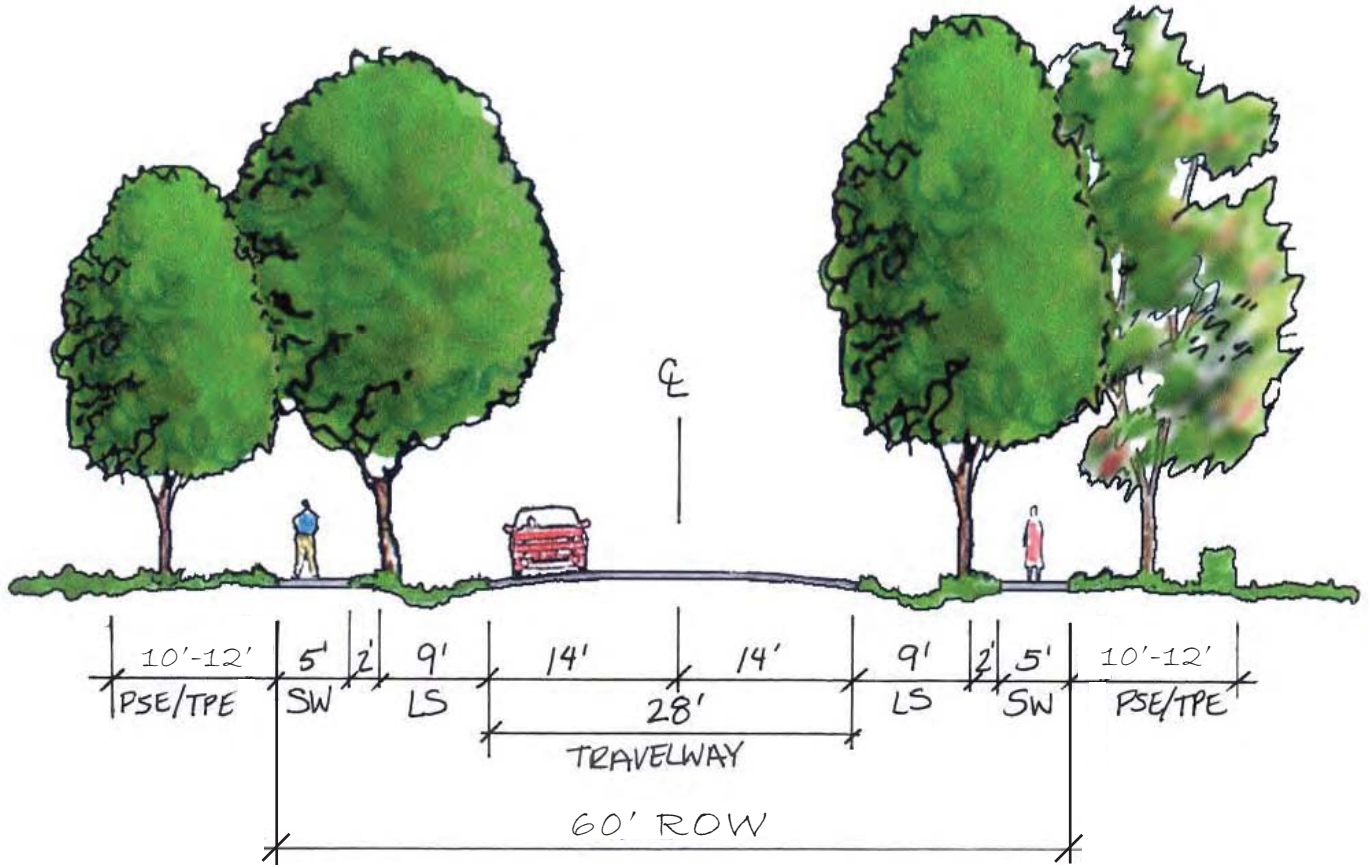
Placement and construction of the EVA route was considered in the Gavilan EIR and approved by the Gavilan Community College District as part of the San Benito Campus Master Plan. Once the campus project moves forward, the Gavilan College District will be installing the EVA route, pending Caltrans approval of an encroachment permit. The Gavilan College District has confirmed (email corr. March 7, 2011) that the proposed EVA route will be constructed in compliance with County and Caltrans conditions of approval. In the event the proposed project preceeds construction of the Gavilan College San Benito Campus and/or the Gavilan College San Benito Campus project is not built, the developer of the Fairview Corners project would be required to construct an alternative EVA route acceptable to the San Benito County Public Works Department and in accordance with County and other applicable standards, prior to the issuance of any residential building permits. (See § 3.14, Traffic and Circulation.)

### **Bicycle and Pedestrian Improvements**

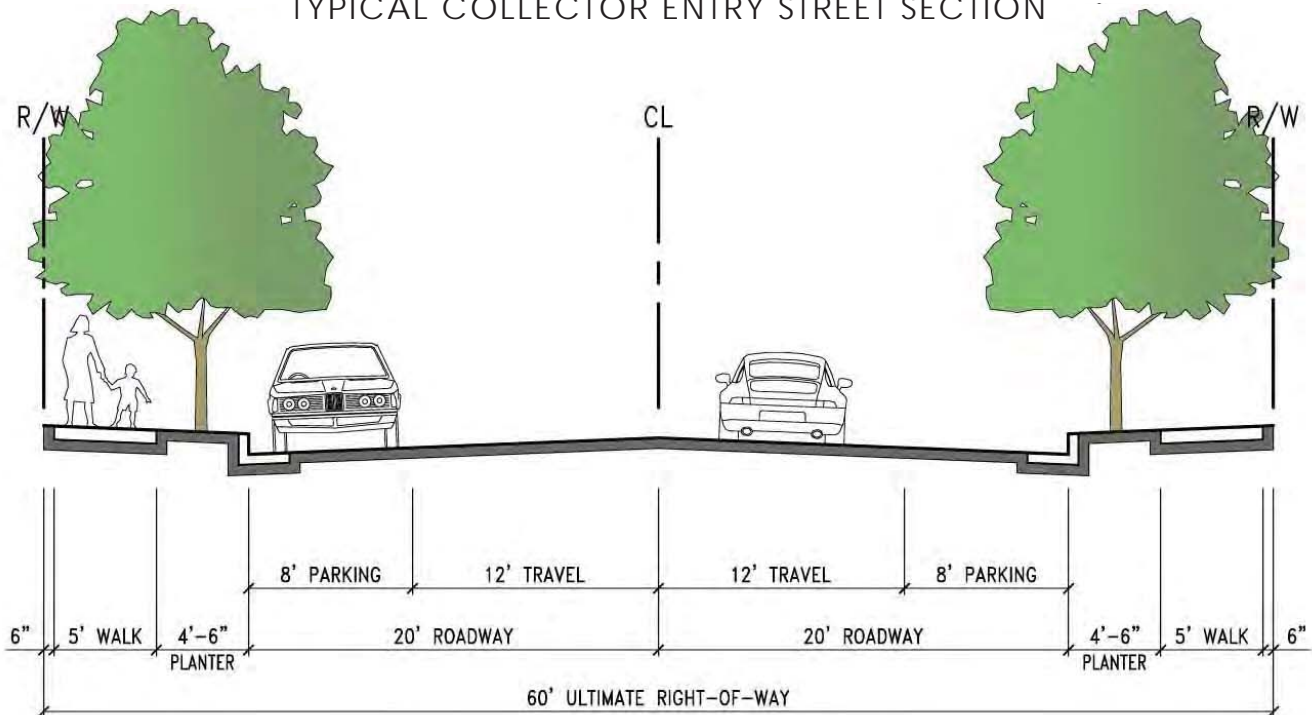
The proposed Specific Plan includes policies for the provision of streets with bicycle lanes and pedestrian walkways, consistent with the County's Bikeway and Pedestrian Master Plan (County Bicycle Master Plan). The project is designed to facilitate the construction and integration of bicycle and pedestrian facilities on- and off-site. The proposed project would include frontage improvements on Fairview Road designed to be consistent with the County's Roadway Design Standards, which would include the provision of bicycle and pedestrian facilities on Fairview Road. Additionally, sidewalks and pedestrian crossings would be provided at the intersection of Fairview Road/Cielo Vista Drive extension to connect the planned on-site pedestrian facilities to existing/future pedestrian facilities on Fairview Road and Cielo Vista Drive.



# Typical Collector Entry and Residential Street Sections



TYPICAL COLLECTOR ENTRY STREET SECTION



TYPICAL RESIDENTIAL STREET SECTION

Not to scale

Source: EMC Planning Group 2010, Ruggeri-Jensen-Azar 2009

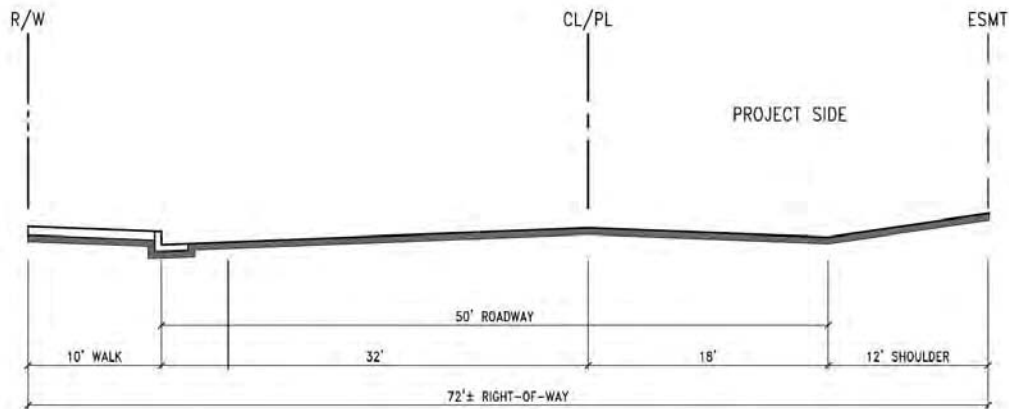
Figure 16

## Typical Collector Entry and Residential Street Sections

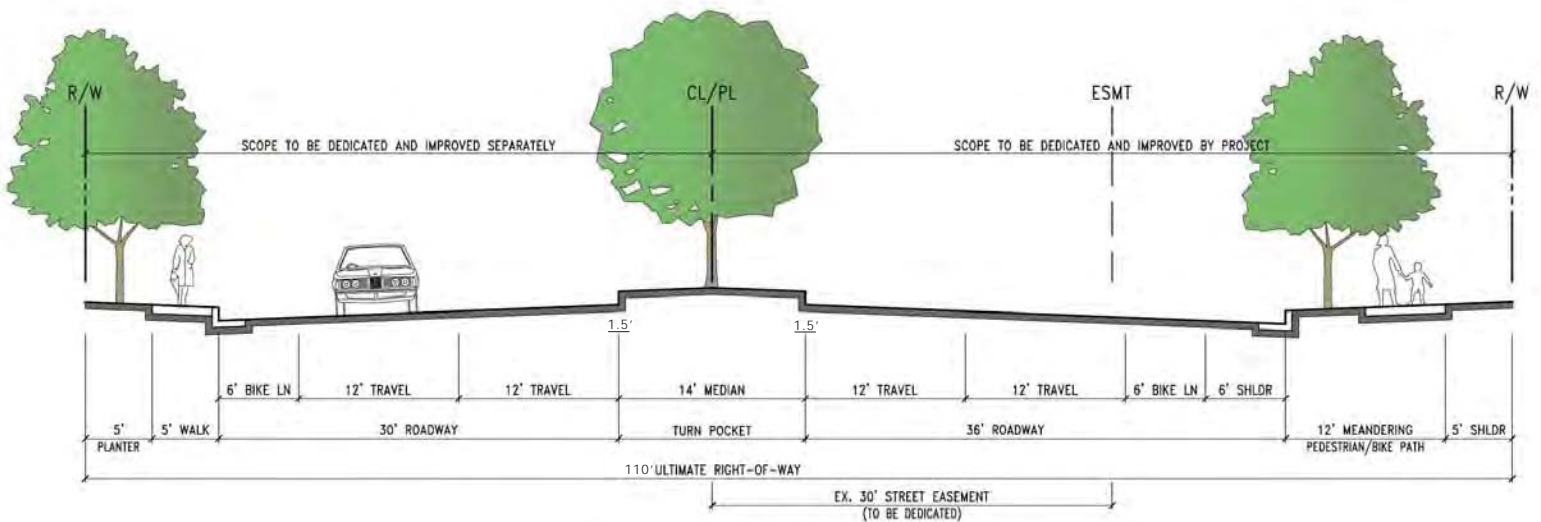
Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

## Existing and Typical Future Fairview Road Section



EXISTING FAIRVIEW ROAD SECTION



TYPICAL FUTURE FAIRVIEW ROAD SECTION

Not to scale

Source: EMC Planning Group 2010, Kier & Wright 2008

Figure 17

## Existing and Typical Future Fairview Road Section

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

The on-site circulation network is designed to integrate pedestrian, bicycle and vehicular modes of transportation, and to maintain open space at the end of cul-de-sacs in order to provide pedestrian and bicycle connectivity within the project site. The project includes a network of internal bike lanes and pedestrian walkways along planned roadways. In addition, the proposed project includes an open space trail system in a loop configuration around the perimeter of or within the project site, which would connect with the adjoining Gavilan College San Benito Campus. Pedestrian circulation routes would be separated from vehicular traffic on all streets, which would contain sidewalks or pedestrian paths. [Figure 18, Bicycle and Pedestrian Connectivity Plan](#), presents the anticipated routes for bicycle and pedestrian facilities. This access network would be connected to the proposed Class I bike lane and pedestrian path along the Fairview Road frontage indicated in the San Benito County Bikeway Master Plan, allowing for convenient access to services and destinations within the Hollister area and to the adjoining Gavilan College San Benito Campus. Enhanced streetscape and landscaped medians, including landscape trees along the Cielo Vista Drive extension, would be constructed as part of the project, which would help create a sense of enclosure and further improve the pedestrian environment.

## **Public Transit**

The developer would work cooperatively with Caltrans, San Benito County and Gavilan College to develop, implement and maintain public transit services to the project site, commensurate with local demand for these services. Appropriate location(s) for a future bus stop on the Cielo Vista Drive extension would be reserved, likely near the Gavilan College San Benito Campus.

## **Parking**

The proposed Specific Plan includes policies to provide at least 1.5 automobile parking spaces for each principal dwelling unit. The project also would provide at least one automobile parking space for each secondary dwelling unit, although this additional parking space may be provided within a common parking lot under certain circumstances. These parking spaces would be provided in accordance with the San Benito County Code Section 25.31.020, except as otherwise indicated in the Specific Plan. In addition, any multi-family developments would be required to provide secure visitor bicycle parking, including one visitor bicycle parking space per five units.

## ***Infrastructure Components and Off-Site Infrastructure***

Proposed water and wastewater infrastructure improvements would be provided to serve the project, and would be phased to coincide with the service and infrastructure needs of individual developments on the project site.

### **Potable Water Service**

The project site is located within the existing service area boundary of the Sunnyslope County Water District (Sunnyslope), which would provide potable water service to the project. Potable water would be delivered to the project site via extension of an existing 12-inch water main, which would run along the west side of Fairview Road. A new domestic water line would tie-in at the intersection of Fairview Road and the Cielo Vista Road extension. This system extension would run along the proposed Cielo Vista Drive extension and distribute water through new lines throughout the residential streets to the units. At the eastern boundary of the project site, the line would be extended outside the project site as a potable water main, which would be connected to the end of an existing six-inch potable water main at the head of the Harbern Way cul-de-sac.

The intent of this connection is to provide a redundant connection point to the Sunnyslope delivery system, to ensure continuous service and adequate fire flows in the event that service from the Fairview Road water main is interrupted. The sizing of the distribution lines would be required to meet Sunnyslope's and other applicable standards for fire and residential flows. See [Figure 19, Potable Water Service Layout](#). Although San Benito County Code Chapter 15.05 contains provisions regarding issuance of permits and standards for private wells, private wells are not proposed as part of the project.

### **Recycled Water Service**

The proposed project would install "purple pipe" to enable recycled water distribution throughout the project site when recycled water becomes available from the City of Hollister's Domestic Wastewater Treatment Plant (DWTP), consistent with the Hollister Urban Area Water and Wastewater Service Master Plan (Hollister Water/Wastewater Master Plan). Based on available information, it is anticipated that recycled water will be available in the future from the planned recycled water system. When recycled water becomes available, recycled water could be used within the project to irrigate certain landscaped areas, subject to compliance with the adopted Hollister Water/Wastewater Master Plan. The operational timeframe for the public facilities for the recycled and distribution facilities is not currently known. However, while the project would be designed to use recycled water when it becomes available, the project has been designed to rely on existing water supplies to meet demand and would not rely on recycled water to serve the project.

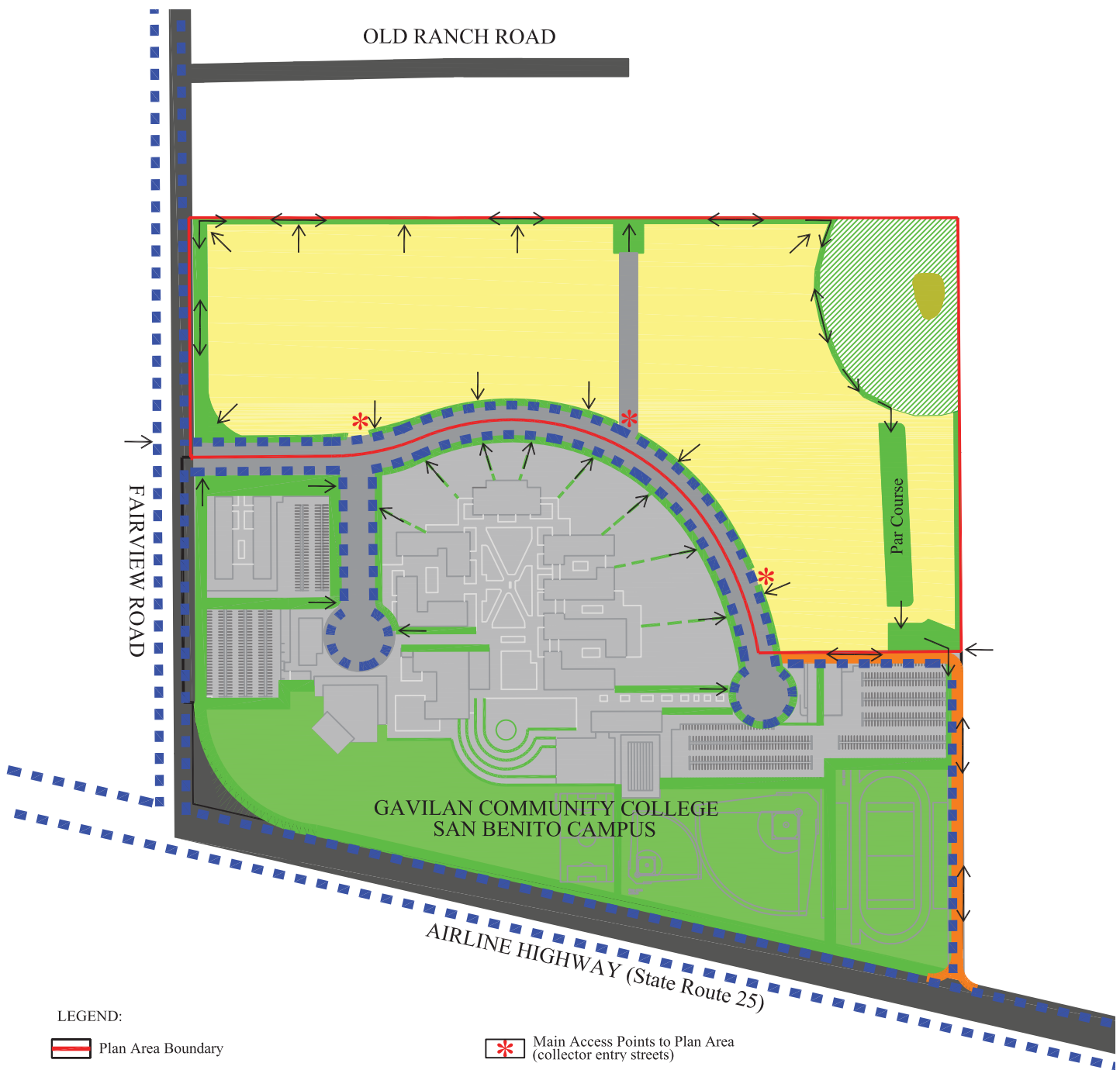


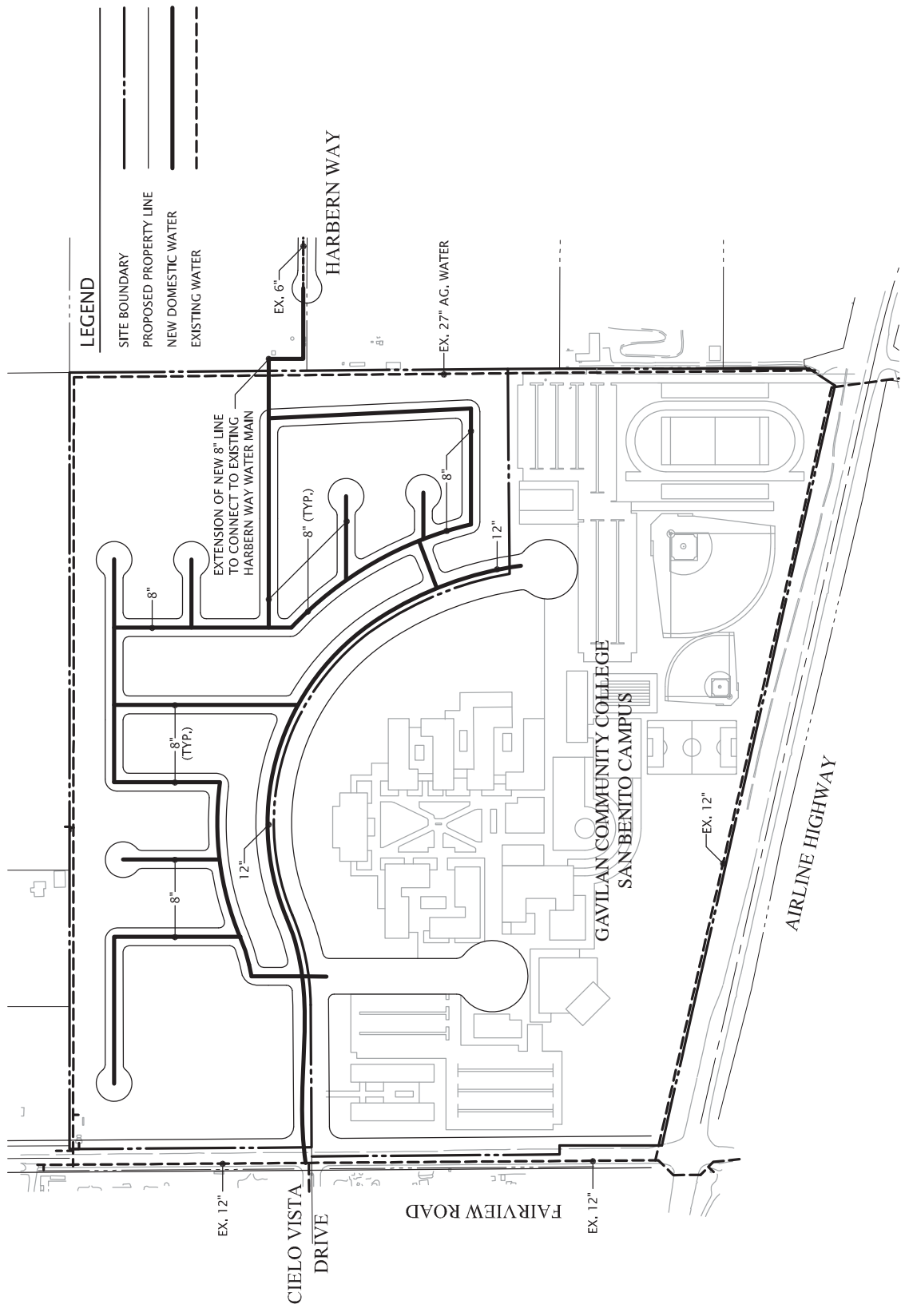
Figure 18

# Bicycle and Pedestrian Connectivity Plan

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*





E M C

Source: EMC Planning Group 2010,  
Kier & Wright 2010

Figure 19

# Potable Water System Layout

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

## Wastewater Service

Wastewater generated within the project site would receive tertiary treatment, as required by the Central Coast Regional Water Quality Control Board. It is anticipated that project wastewater would be treated through a connection to the DWTP in accordance with the adopted Hollister Water/Wastewater Master Plan. The proposed project would allow the use of septic systems on parcels with a minimum size of one acre, and if otherwise consistent with County and City of Hollister regulations and design criteria and other applicable laws and regulations; however, the proposed project intends to connect to the DWTP for treatment. The recently expanded DWTP is operational, and its planned capacity can accommodate the project site's estimated wastewater flows, as further described in Section 3.14, Wet and Dry Utilities and Energy.

There are two options to convey the project's wastewater to the DWTP. [Figure 20, Wastewater Conveyance Options](#), presents the locations of potential conveyance options. Both options would be gravity fed systems that would connect to the existing sewer main on Enterprise Road, west of the site. Neither option requires the construction of pumping or lift stations to convey the wastewater to connect to existing wastewater infrastructure.

"Option 1" is to construct a new sewer main across Fairview Road, which would connect to the existing system at the intersection of Fairview Road and the Cielo Vista Drive extension. This connection would discharge the wastewater from the project site through the existing 8-inch main to the western end of the Cielo Vista Residential subdivision. This main would contain sufficient capacity to serve the project site, and also would accommodate flows from the existing Cielo Vista Residential subdivision. From Enterprise Road, wastewater would flow within the City of Hollister's existing wastewater lines to the DWTP.

"Option 2" is to construct a new sewer main within Fairview Road running south to Airline Highway. From here, a new main would be constructed within Airline Highway and Enterprise Road to serve existing and approved, but not yet developed, projects along Airline Highway, as well as the project site. A new main would then be constructed within Enterprise Road solely for the project site, connecting it to the existing sewer system within Enterprise Road. From Enterprise Road, wastewater would flow within the City of Hollister's existing wastewater lines to the DWTP.

Under both options, the sewer lines would be required to be sized in accordance with the applicable standards of the County and the Hollister Wastewater Master Plan.

## Stormwater Drainage

Under project conditions, assuming maximum buildout of 220 units, the project site would substantially maintain the existing drainage patterns on the site. The topography of the site exhibits gradual slopes that naturally drain toward the northeast and to the west, and a small

portion to the south. It is anticipated that drainage from the project site, once developed, will maintain the two main flows toward the northeast and west. It is anticipated that stormwater flows for these drainage areas would be collected by a network of curbs and gutters, storm drains, bioswales, and retention pond(s), with a primary retention pond placed in the location of the former stock pond, so long as the pond is determined by the relevant regulatory agencies to not be required for habitat mitigation. There are also Specific Plan policies that provide for the sharing of drainage capacities between the project and the adjoining Gavilan College San Benito site, so long as certain conditions are satisfied. In any event, the proposed project would be required to design the capacity of stormwater retention facilities to be based on 100-year storm events and to be of adequate size to retain and infiltrate all stormwater on the site to pre-development levels as required by County standards (San Benito County Code, Title 23, Chapter 23.17, Section 23.17.003(B) and Chapter 23.31, article III, section 23.31.040). The proposed project also would incorporate, to the extent feasible, low impact development (LID) features, including directing drainage from impervious surfaces to bioswales for infiltration, utilizing biotreatment (natural pollutant filtering) where stormwater runs off of paved surfaces, and capturing roof drainage for reuse as irrigation. These LID features would help to further reduce flows and prevent urban pollutants from entering the County's drainage system and migrating to off-site areas. [Figure 21, Conceptual Drainage Plan](#), presents the preliminary drainage plan for the site.

## ***Solid Waste Collection***

Solid waste generated by the project would be sent to the existing John Smith Landfill, located approximately two miles from the project site. The San Benito County Integrated Waste Management Department is responsible for oversight of landfill operations in the unincorporated San Benito County. Nor Cal Waste Systems would provide solid waste, recycling and yard waste collection in the unincorporated County, including the project site.

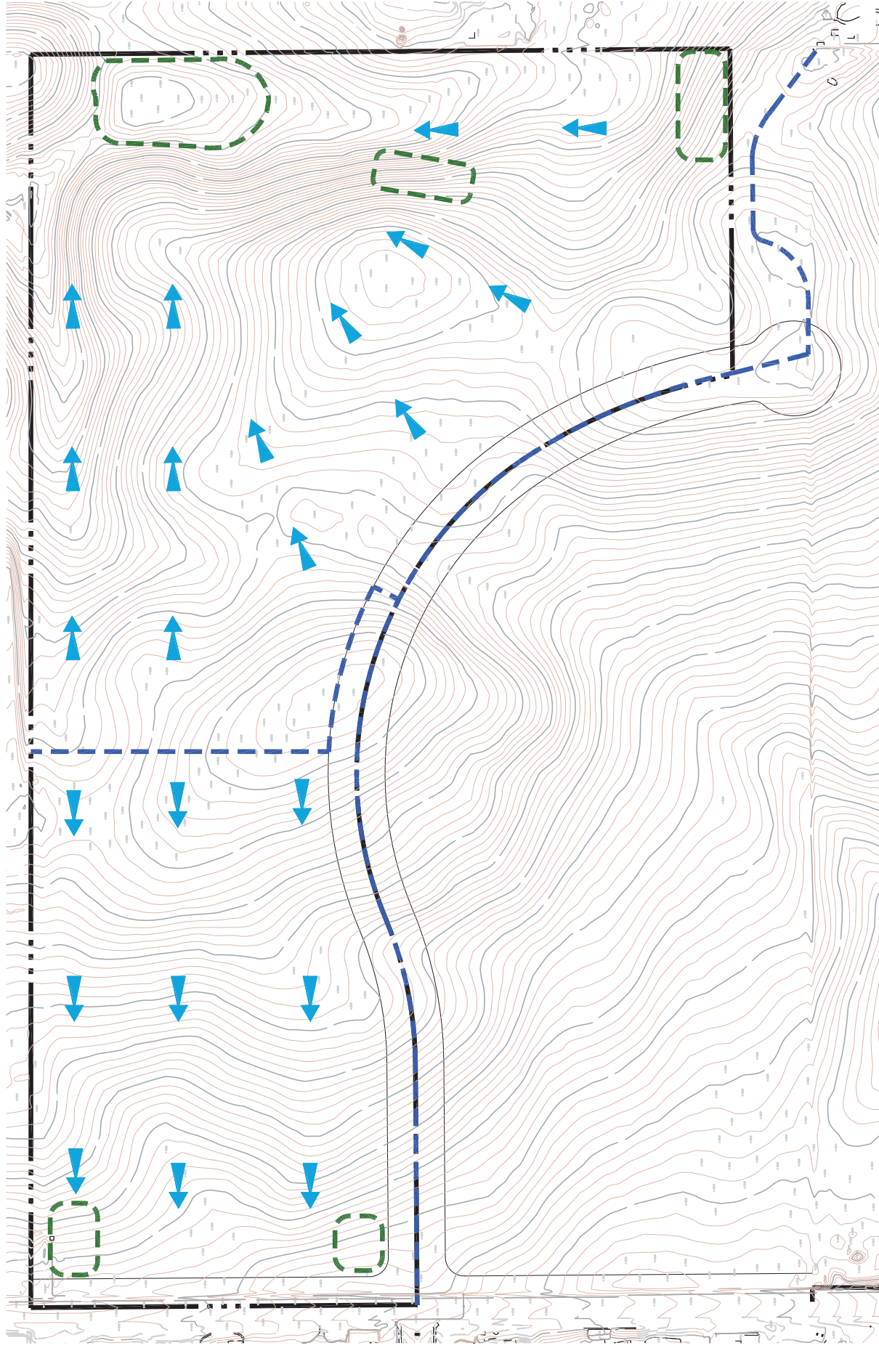
## ***Grading***

Grading and fill would be required to prepare the project site for the construction of roadways and utility infrastructure. Grading is also required to provide drainage, level building sites, and to accommodate retention facilities, including an anticipated basin near the former stock pond in accordance with any resource agency requirements. This Draft EIR assumes that the grading for the two main roadways (Fairview Road and the Cielo Vista Drive extension), the backbone infrastructure, and the drainage improvements would occur during the first phase of the project. However, the project may be implemented over a period of five to 16 years as identified below. The proposed project would balance cut and fill areas on the site to avoid the need to import or export soils. The general effect of proposed grading to the site topography is presented in [Figure 22, Conceptual Cut and Fill Diagram](#).





*This side intentionally left blank.*



0 300 feet

PLAN AREA BOUNDARY  
PROPOSED DRAINAGE AREA BOUNDARY  
POTENTIAL STORMWATER COLLECTION AREA  
DIRECTION OF FLOW

Source: EMC Planning Group 2010,  
Ruggieri-Jensen-Azar 2009

Figure 21

# Conceptual Drainage Plan

Fairview Corners Residential Specific Plan EIR

E

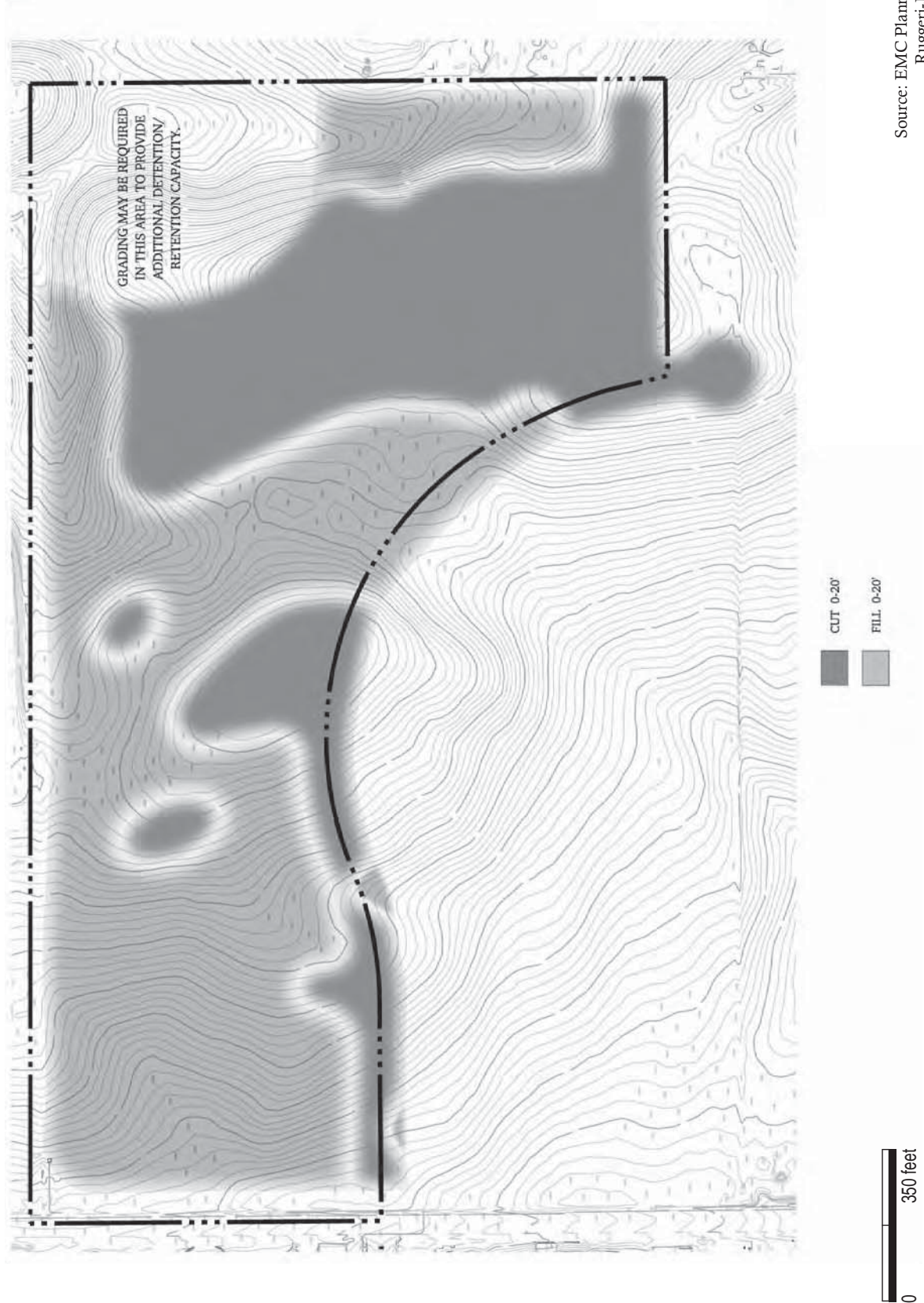
M

C



*This side intentionally left blank.*





Source: EMC Planning Group 2010,  
Ruggeri-Jensen-Azar 2010

Figure 22

# Conceptual Cut and Fill Diagram

Fairview Corners Residential Specific Plan EIR

*This side intentionally left blank.*

## ***Other Utilities***

The project would be served with electricity and natural gas by Pacific Gas & Electric (PG&E); all new utility lines would be undergrounded within the project site.

Land line phone service to the project would be provided by AT&T. Cell phone service to the project's residents would be provided from a variety of available service providers. Charter Communications would provide cable service to the project, as well as cable access to the Internet. Other internet providers are also available for dial-up, cable or network service.

## ***Police, Fire and Emergency Medical Services***

The San Benito County Sheriff's Department would provide law enforcement services to the project. The San Benito County Fire Department would provide fire service to the project, which is operated under contract with Cal Fire. The San Benito County Department of Emergency Services would oversee emergency medical response services to the project.

## ***Schools***

It is anticipated that school-age children who live in the project would attend the existing Cerra Vista Elementary School and San Justo Middle School. Older children would attend the existing San Benito High School, although it is anticipated that the district will construct a new high school to serve the area, including the project site, at some point in the future.

## ***Phasing***

Total project build-out is anticipated to occur in phases. The proposed project would be constructed in at least two major phases (Phase I and Phase II), over a period of five to 16 years. The two major phases would accommodate grading and infrastructure construction and may be divided into additional "sub" phases, each of which may last approximately two years, with up to four sub-phases within each major phase (Phase I (A-D) and Phase II (A-D)).

The Specific Plan conceptually discusses the various project components; the ultimate site plan and related phasing for individual developments on the project site would be more precisely defined through the first subdivision map application process. The development phases are expected to occur sequentially (Phase I, then Phase II) although the phases may occur concurrently. Development of each phase shall include all infrastructure, services, facilities, and amenities, both public and private, needed to serve the uses and structures within that phase, which would be completed in accordance with the applicable provisions of the Specific Plan and relevant master plans, as discussed further below.

Figure 23, *Preliminary Phasing Plan*, presents the conceptual phasing plan. It is anticipated that Phase I would contain the backbone access improvements to serve the proposed project. Development would occur first near Fairview Road, with successive phases progressing eastward across the site. It is anticipated that grading, drainage, and roadway backbone infrastructure would be completed during Phase I.

The project developer would be required to prepare an Infrastructure Phasing Master Plan, to be approved by the County during the first subdivision map application process. All development within the project must be consistent with the approved Infrastructure Phasing Master Plan (as it may be amended).

## ***Development Scenarios Studied in this Draft EIR***

Because of the inherent site plan flexibility as set forth in the Specific Plan, the ultimate unit count, density and land plan would be finalized as part of the subsequent subdivision map process, in accordance with applicable standards and requirements including, without limitation, a maximum unit count of 220 primary dwelling units. Accordingly, for purposes of environmental review, this Draft EIR evaluates those development scenarios that would result in the most significant environmental impacts to ensure a conservative analysis, as further described below.

### **Maximum Build-Out Scenario Evaluated For All Environmental Topics**

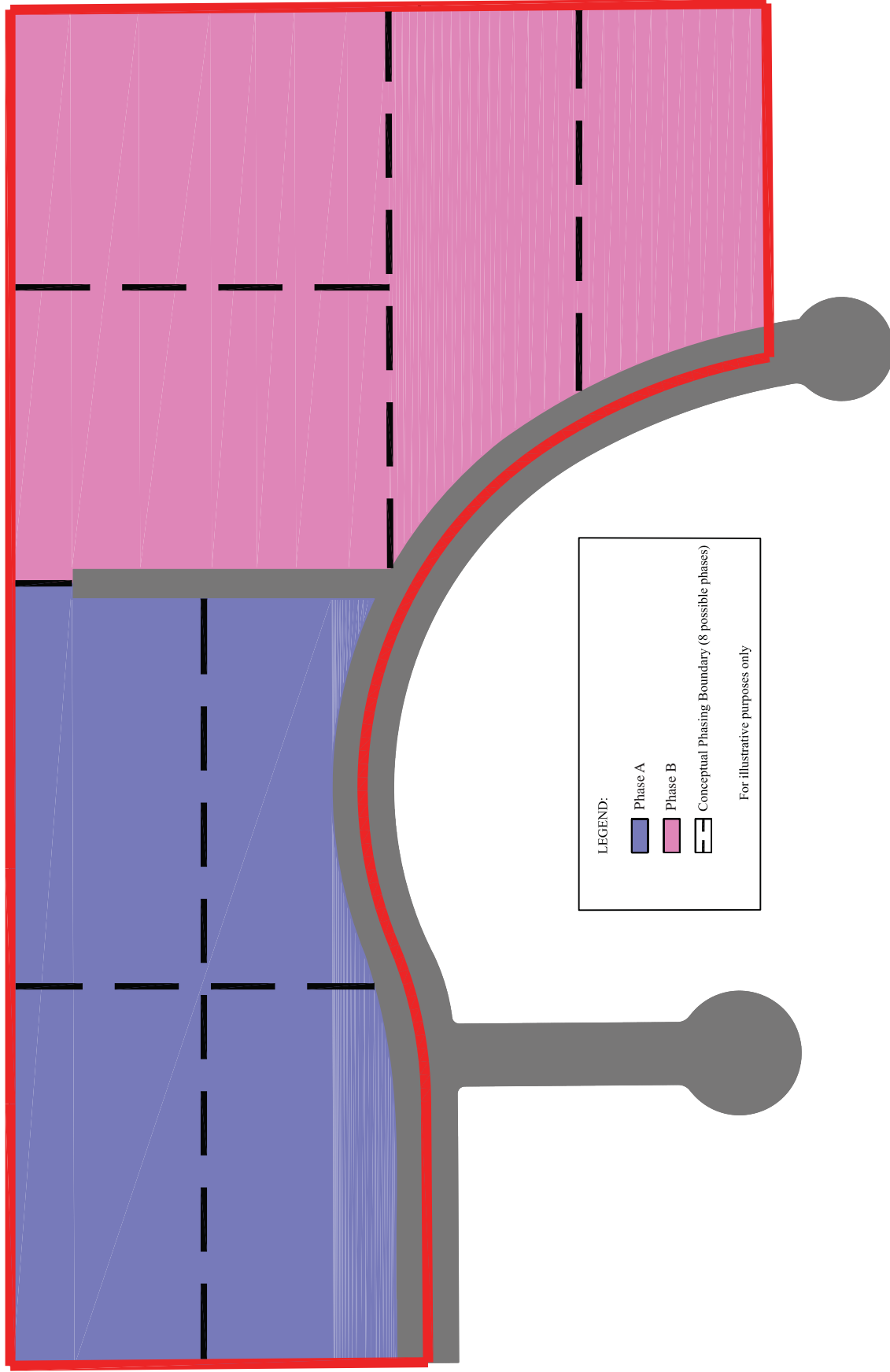
For all environmental topics studied herein, this Draft EIR evaluates the potential environmental impacts that could occur if the maximum number of primary dwelling units — 220 — were built, with all of these units developed as one- or two-story, single-family, detached homes on approximately 4,000-square-foot to 12,000-square-foot lots.

### **Additional Scenarios Evaluated For Specific Environmental Topics**

Following is a brief summary of the additional development scenarios studied in this Draft EIR in connection with certain environmental topics to ensure that all potential impacts of the project are fully evaluated.

#### **Aesthetics**

In addition to the maximum build-out scenario, the Draft EIR analyzes development of 220 units, where a portion of those units (75) would be built as multi-family housing in apartment complexes, as opposed to one- or two-story single-family ranchettes (See Figure 11, Conceptual Lotting Plan B, as an example). The Draft EIR analyzes the visual impact of locating these multi-family units in the western portion of the project site, adjacent to Fairview Road, or in other locations on the site, should market demand and any affordable housing requirements indicate that multi-family units are needed.



Source: EMC Planning Group 2010  
For illustrative purposes only

Figure 23  
**Preliminary Phasing Plan**  
Fairview Corners Residential Specific Plan EIR



*This side intentionally left blank.*

## Noise

In addition to the maximum build-out scenario, the Draft EIR analyzes the potential noise impacts associated with development of 220 units, where a portion of those units (75) are built as multi-family housing in apartment complexes along Fairview Road.

## Wet and Dry Utilities and Energy

In addition to the maximum build-out scenario, this Draft EIR evaluates whether the effects of septic systems, if allowed for the proposed project, would result in groundwater quality impacts or in significant impacts to wastewater services.

## Gavilan College San Benito Campus Emergency Vehicle Access (EVA) Route

The proposed project intends to utilize the Airline Highway EVA route planned for by the Gavilan College San Benito Campus. As such, the proposed project does not include construction of the EVA access route; instead, this route (refer to Figure 14, Circulation Diagram) is anticipated to be constructed by the College District. However, in cooperation with the College District, this Draft EIR evaluates potential hazards and potential impacts to cultural resources and biological resources resulting from construction within the Airline Highway EVA, which were not analyzed on a project level in the Gavilan EIR. This analysis will be used by the College District in its application for the Caltrans encroachment permit needed for the planned San Benito Campus EVA. The location of the proposed Airline Highway EVA is shown in [Figure 24, Off-Site Study Areas](#).

## 2.4 PROJECT OBJECTIVES

Consistent with CEQA Guidelines Section 15124(b), a clear statement of objectives and the underlying purpose of the project shall be discussed. Following is a statement of the project objectives based on information provided by the project applicant:

- Implement the intention of the San Benito County Board of Supervisors when the Board designated the Plan Area an Area of Special Study to allow higher density development.
- Create a mutually supportive relationship between the Plan Area and the adjoining future Gavilan College San Benito Campus site that integrates connections, shares facilities and infrastructure, and collaborates on mitigation where appropriate.
- Provide a maximum residential unit count of 220 for the creation of housing opportunities in proximity to existing utilities and infrastructure improvements.



- Provide for a mix of housing types close to potential employment opportunities and students at the adjacent future Gavilan College San Benito Campus, public transportation, public facilities, and goods and services that will meet the needs of a variety of households.
- Provide for an Affordable Housing Program that encourages secondary units, and collaborates, where feasible, with the Gavilan College San Benito Campus.
- Provide a range of potentially mixed residential housing opportunities that will meet the needs of a variety of households with lot sizes ranging from 4,000 square feet to five acres, with a range of housing types and square footages.
- Provide convenient pedestrian connections and recreational opportunities through the provision of pocket parks, open space area, corridors, and connections with the adjacent future Gavilan College San Benito Campus site.
- Minimize the noise and speed of traffic to ensure the safety of residents through the design of cul-de-sacs and curvilinear streets.
- Provide employment opportunities resulting from infrastructure improvements and residential construction.
- Create a revenue neutral planned community where long term operation and maintenance costs are paid for by one or more of several potential funding options or appropriate financing district mechanisms.

## 2.5 PROJECT RELATIONSHIP TO EXISTING PLANNING DOCUMENTS

### ***General Plan***

Government Code Section 65300 requires that each planning jurisdiction (i.e., city or county) prepare, and the legislative body adopt, a comprehensive, long-term general plan for the physical development of the community. The general plan is a comprehensive document that covers many aspects of land use including development, density, circulation, resource preservation and safety. The San Benito County General Plan is the foundational planning document for San Benito County, and provides a comprehensive, long-term plan for the physical development within the County.





Source: Kier & Wright 2009  
Google Earth 2010

Figure 24  
Off-site Study Areas

*This side intentionally left blank.*

The project site currently has a General Plan land use designation of “Rural.” On June 12, 1989, the San Benito County Board of Supervisors adopted Resolution No. 89-21 designating approximately 758 acres east of Fairview Road, including the project site, as an “Area of Special Study.” This designation is described in the General Plan as follows:

This designation applies to areas of the County where a combination of the following criteria apply: 1) More concentrated development than is presently allowed may be desirable provided a comprehensive plan for public service and resource conservation is integrated in order of priority into a specific plan, community plan or area plan. 2) As a result of prior, piecemeal subdivisions, a plan for integrated development and coordination of governmental services and/or community facilities is desirable. 3) The County has identified the area as a potential area for commercial, industrial, and/or residential development.

The intent of the Area of Special Study designation is to direct development from natural resources and environmentally hazardous areas, to effectively plan the design, development, and financing of services and to develop open space programs. General Plan Amendments (GPAs) would be required to implement the project, including changing the current land use designation of Rural to “Fairview Corners – Specific Plan (“FVC – SP”); a new proposed land use designation that would allow development of the project as proposed in the Specific Plan. Additional specified conforming GPAs also are proposed to ensure consistency between the project and the General Plan.

## ***San Benito County Code***

The San Benito County Code governs development of the project, except as those regulations may be amended or supplemented by the Specific Plan, as proposed as part of the project.

## ***Zoning Ordinance***

The County’s Zoning Ordinance (title 25 of the San Benito County Code) implements the policies of the General Plan by classifying and regulating the land uses and associated development standards within the County. As discussed further below, while the Zoning Ordinance establishes the specific uses and development regulations for land uses for the County generally, a specific plan may include development standards tailored for a specific geographic area within the County.

The current zoning designation for the project site is “Rural (R),” with a minimum parcel size of five acres, which corresponds with the General Plan land use designation of Rural. A zoning map amendment would be required to change the project site’s existing zoning designation to a new zoning designation, “Fairview Corners – Specific Plan (FVC – SP),” indicating that the project site is designated for development approval through the specific plan process. The Fairview Corners Residential Specific Plan would set forth the zoning for the project site.

## ***Specific Plan***

A specific plan is a regulatory and planning document that may be used to implement development in a localized area. While the general plan is the primary guide for growth and development in a community, a specific plan may specify planning processes and land use regulations for a particular geographic area, so long as it is consistent with the general plan.

The Fairview Corners Residential Specific Plan is intended to provide the comprehensive vision for development of the project. It is intended to generally work in concert with the existing regulatory structure of the County, except as may be amended or supplemented therein.

As explained more fully in Section 3.9, Land Use and Planning, the Specific Plan establishes the permitted, conditionally permitted, and prohibited uses, as well as the physical development standards and design guidelines for the project. The project would require adoption of the Specific Plan, upon which the project site would be designated under the General Plan and rezoned to “Fairview Corners – Specific Plan (FVC - SP).” Thereafter, development of the project would be governed in accordance with the Specific Plan and other applicable local, state, and federal law.

An analysis of the proposed project’s consistency with relevant General Plan policies is contained within each of the topical sections in Chapter 3.0, Environmental Setting, Impacts, and Mitigation Measures and Section 3.9, Land Use and Planning.

## **2.6 REQUESTED ACTIONS AND REQUIRED APPROVALS**

As required under CEQA, this EIR provides the environmental information and analysis necessary for the decision makers and the public to adequately consider the effects of the requested development proposal. San Benito County, as the lead agency, has approval authority and responsibility for considering the environmental effects of the proposed project as a whole. In order to implement the project, a development application has been submitted to San Benito County. The project would require the following discretionary approvals from San Benito County:



- Approval of an amendment to the San Benito County General Plan to change the General Plan Diagram to show the project site as “Fairview Corners – Specific Plan (FVC–SP)”; and to make any other specified conforming amendments to the General Plan to ensure consistency.
- Adoption of the Fairview Corners Residential Specific Plan.
- Approval of amendments to the San Benito County Code to change the text to reflect the new zoning designation as “Fairview Corners – Specific Plan (FVC–SP)”; and to make any other conforming amendments necessary to ensure consistency between the County Code and the project.
- Approval of the Fairview Corners Residential Specific Plan as the applicable zoning for the project.
- Approval of a development agreement between the County and the project applicant.
- Approval of parcel, tentative, and/or final subdivision maps for specific areas of development within the project site.
- Approval of grading permits.
- Approval of conditional use permits and design review consistent with the processes set forth in the Fairview Corners Residential Specific Plan.

In addition, the project may require discretionary approvals, permits, and entitlements from other federal, state, and regional agencies including:

- Sunnyslope County Water District
  - Approvals related to water connections
- California Department of Fish and Game
  - Potential Take Permit - California tiger salamander
- Central Coast Regional Water Quality Control Board
  - Coverage under the Statewide NPDES General Permit for Stormwater Discharges Associated with Construction Activity
- Caltrans District 5
  - Encroachment Permit (State Route 25 - Airline Highway)

## 2.0 PROJECT DESCRIPTION

- U.S. Fish and Wildlife Service
  - Potential Take Permit – California tiger salamander
- San Benito Local Agency Formation Commission
  - Potential LAFCO action required for the proposed recycled water and wastewater services provision to the project.